

GTS Project Noise Wall Design Community Advisory Committee

Bloor St. to St. Clair Ave.

Meeting # 4
October 21, 2013



About this meeting

6:30 p.m. - Welcome

6:35 p.m. - Recap of CAC process

6:50 p.m. - What you told us and what we did

7:00 p.m. - What your neighbours told us

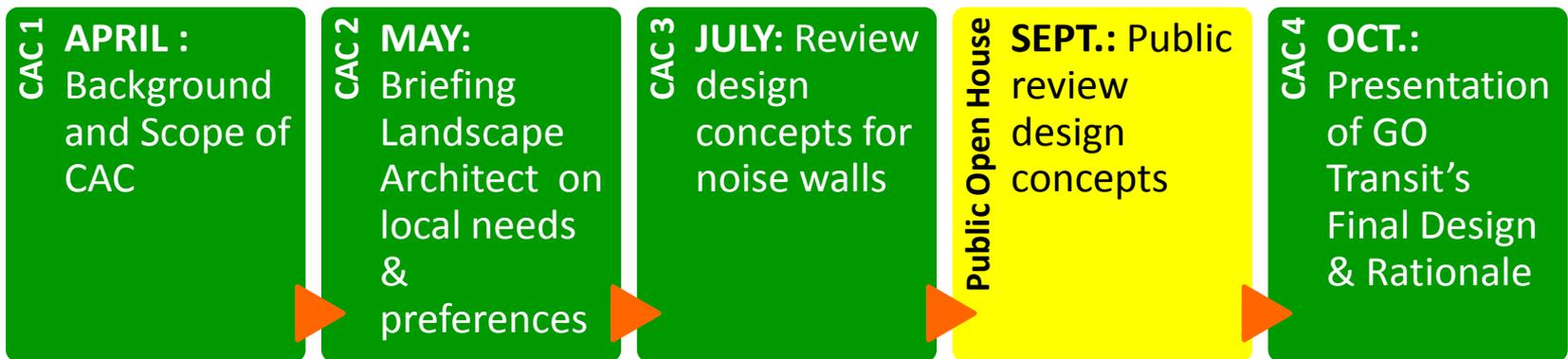
**7:10 p.m. - Presentation of Final Noise Wall Designs
and Q&A**

8:00 p.m. - Next steps

Thank You

- Thank you for volunteering your time since April to help inform the design of noise walls in your neighbourhood.
- Your thoughtful feedback has been invaluable to the design process.
- We continually want to improve our engagement practices. Please take the time to complete the feedback form on the overall consultation process.

Recap of CAC Process



Our Commitment for 2015

- Install noise walls required for Full Build service in 2015.
- If the committee reasonably feels that the wall design does not address their concerns, GO Transit will consider deferring installation of Full Build service mitigation in 2015.
- The community, elected officials, and GO Transit will need to agree on a process that allows the wider community to fairly determine a recommended direction.

What you told us

Dupont St. to St. Clair Ave.



- Greening; noise reduction; graffiti prevention were priorities
- “Is there an opportunity to create spaces and places that would improve the industrial feel of the area?”
- Improve green space behind 18 Hook Ave. and preserve view of buildings.
- Use graffiti as art. Opportunity to do something creative and green.
- Committee agreed landscaping around WTRP is a priority over this area.

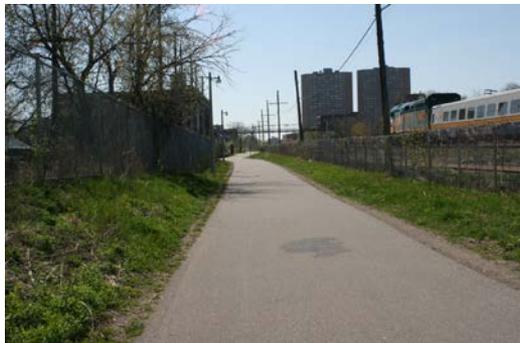
What you told us



Dundas St. W. – Dupont St. to Bloor St.

- Greening and landscaping; noise reduction; sunlight; views; safety
- Minimize loss of green space along Dundas St. West as the street abuts the corridor land.
- Preserve railside garden and trees, replace green canopy, prevent vehicle noise reflection off of walls, preserve view across the corridor and downtown.

What you told us



West Toronto Railpath - Cariboo St. to Bloor St.

- Preserve greenery and current sun/shade patterns, preserve views across the corridor and improve public gathering spaces.
- Deter graffiti and noise reduction
- Shared concern for the loss of trees, birds and habitat.
- The areas at ends of Ernest, Wallace and Ruskin streets have “evolved as community nodes” and need noise reduction, sunlight, greenery and seating provision.

How This Influenced design

- In areas where views and sunlight are a priority, clear or coloured acrylic panels will provide the the best solution while still reducing noise by reflecting back to the tracks
 - Along and adjacent the West Toronto Railpath and along Dundas St. West.
- In areas where maximum noise absorption is a priority, precast solid textured panels will provide the best solution while also deterring graffiti.
 - In the industrial area towards St. Clair Ave West; around the CP and GO rail junction; and either side of the Bloor GO station.

What Your Neighbours Told Us

- Over 200 people attended the 4 public meetings corridor wide.
- We received 118 comments during our public consultation period which ran from September 10 to October 2.
- General themes included the need for noise mitigation, preservation of views and light and preserving the history of the neighbourhoods along the corridor.

What Your Neighbours Told Us

Ruskin / Wallace / Macaulay Avenues

- Question why there was no noise wall in the current plan and if needed, they should be a continuum of the coloured wall. Concern about the design being left to the discretion of the property developer for that site.
 - We will inform the developer of our final plans and work with the developer to make available the information of our suppliers.

Perth Ave.

- Preference for a hybrid wall: 2m max concrete and remaining clear transparent wall.

What Your Neighbours Told Us

Dundas St. West: Dupont St. to Bloor Ave. and Railside Garden

- Preference for clear transparent walls, liked for preserving natural light, blending into the neighbourhood and keeping the railside garden intact and allowing some successfully established plants to flourish. Support for this to be built for 2015 due to increased rail traffic.
- One response suggested a mix of full transparent panels at street ends between Dupont and Glenlake, with hybrid neutral coloured concrete vine walls in between. Also suggested art/event panels in one or two small sections. The transparent panel at Glenlake should be sufficiently wide to ensure a view of the Wallace pedestrian bridge.
 - For CAC consideration

What Your Neighbours Told Us

Railpath

- Clear transparent walls preferred over the yellow and blue transparent walls, to let in natural sunlight and deter crime.
- Other responses felt neither transparent nor concrete designs fitted with the railpath's urban park landscape and preferred a grassed berm. Fears that the transparent wall would create a dead zone that is baked hot by the summer sun reflecting off the plastic panels.
- The railpath is natural and green.
- Coloured panels not liked for following reasons:
 - Cast coloured light onto the railpath, natural light is preferred
 - Colours will fade and be less vibrant
 - Clear transparent panel preferred
 - Clear acrylic instead of coloured acrylic is for CAC consideration

What Your Neighbours Told Us

Transparent Walls

- Preference for clear transparent walls rather than the coloured, yellow/blue panels. Clear transparent with limited concrete base liked for giving the best aesthetic affect for the area. Reasons given included:
 - People enjoying natural light on the railpath
 - Colour panels drastically altering the natural beauty of the railpath
 - Concern about discolouration, fading and streaks
 - Some concern that sunlight or car headlights would create reflected light
 - Clear acrylic instead of coloured acrylic is for CAC consideration.

What Your Neighbours Told Us

Green Vine Wall

- Preference to maximize throughout the corridor and request for evergreen shrubbery and fast growing vines to help buffer the sound of trains and create a better aesthetic.
 - We will be planting invasive vine species approved by the Toronto and Region Conservation Agency and will be working with the City of Toronto to identify opportunities for greening in front of the walls.
- Concern expressed that some wall sections in heavy and constant solar shade and/or exposure to NW-north-NE wind will not support vegetation.
 - We conducted a solar study and we have investigated local examples of vine growth in our climate.
- Spacing of the vine supports too wide to provide complete greening.
 - Metrolinx has noted this concern and will finalize spacing during detailed design.

What Your Neighbours Told Us

Concrete Wall

- Railside-facing concrete walls – suggestion to use art walls to discourage graffiti. Seen as a negative addition to the urban fabric, with no discernible public benefit and would create a prison feel.
 - There is an art panel designated between Humberside Ave. and Dupont St.

Film Strip

- A few comments on this design disliked it for appearing dated or 'prison like'.
 - The filmstrip colours have been refined.

What Your Neighbours Told Us

Bird Safety

- Concern over transparent walls and bird strikes.
 - All acrylic panels will have bird “fritting” to reduce strikes.

Art Panel

- Request for no corporate or commercial content on the art and event walls.
 - We will work with the community to explore options, including existing art programs in order to identify artists for murals or other artwork. We will also work with local BIAs, stakeholders and other community organizations to take ownership of the event panels.

What Your Neighbours Told Us

Overall Design Comments

- Feedback frequently requested that greenery and transparency be maximized throughout the corridor and concrete minimized.
- Design priorities described as visibility; safety; greenery and effective maintenance
- Some responses described them as well designed given the current parameters.
- Some responses described the designs as cheap and disappointing.

Design Examples in Your Community



Design Examples in Your Community



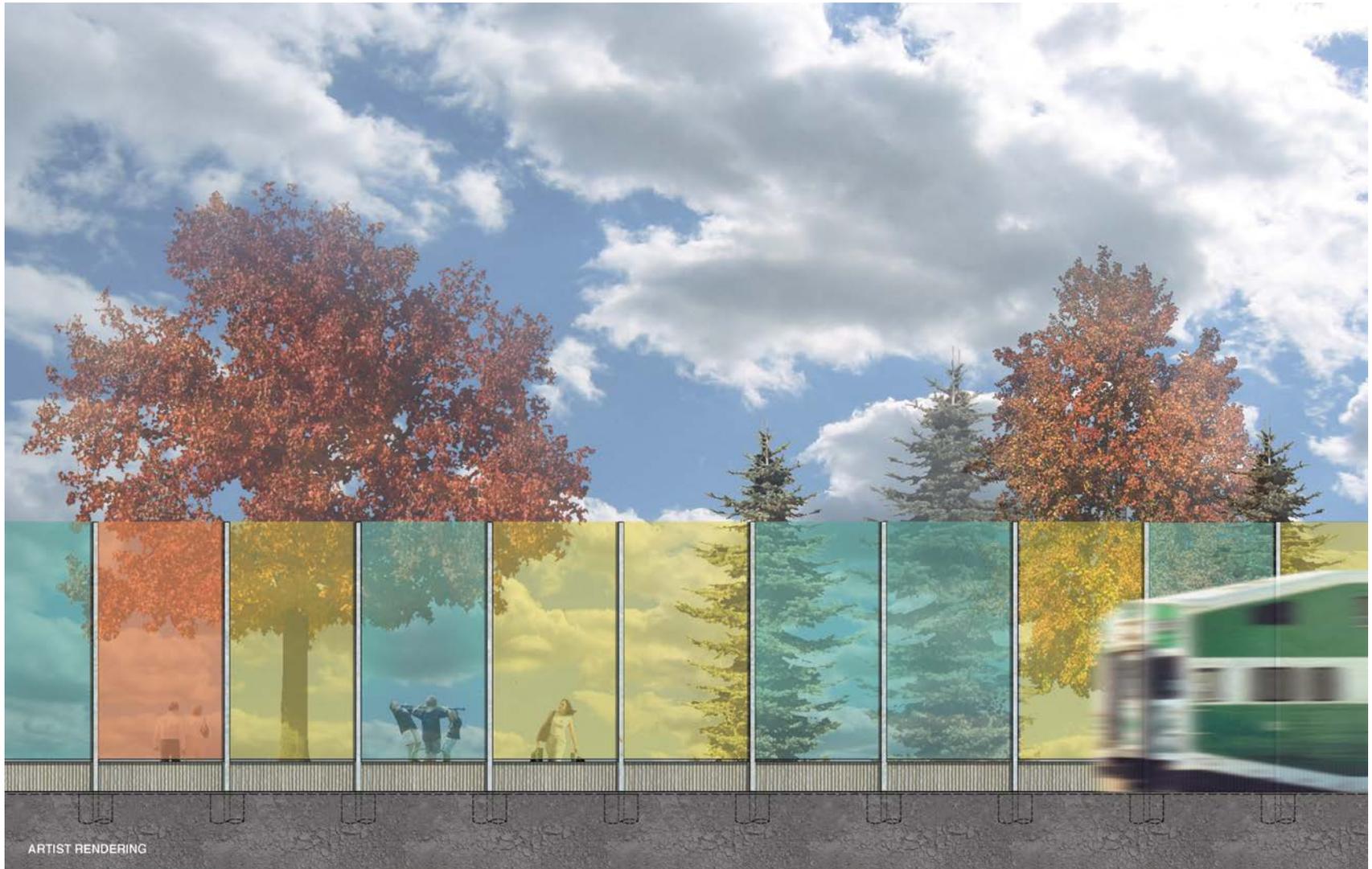
Design Examples in Your Community



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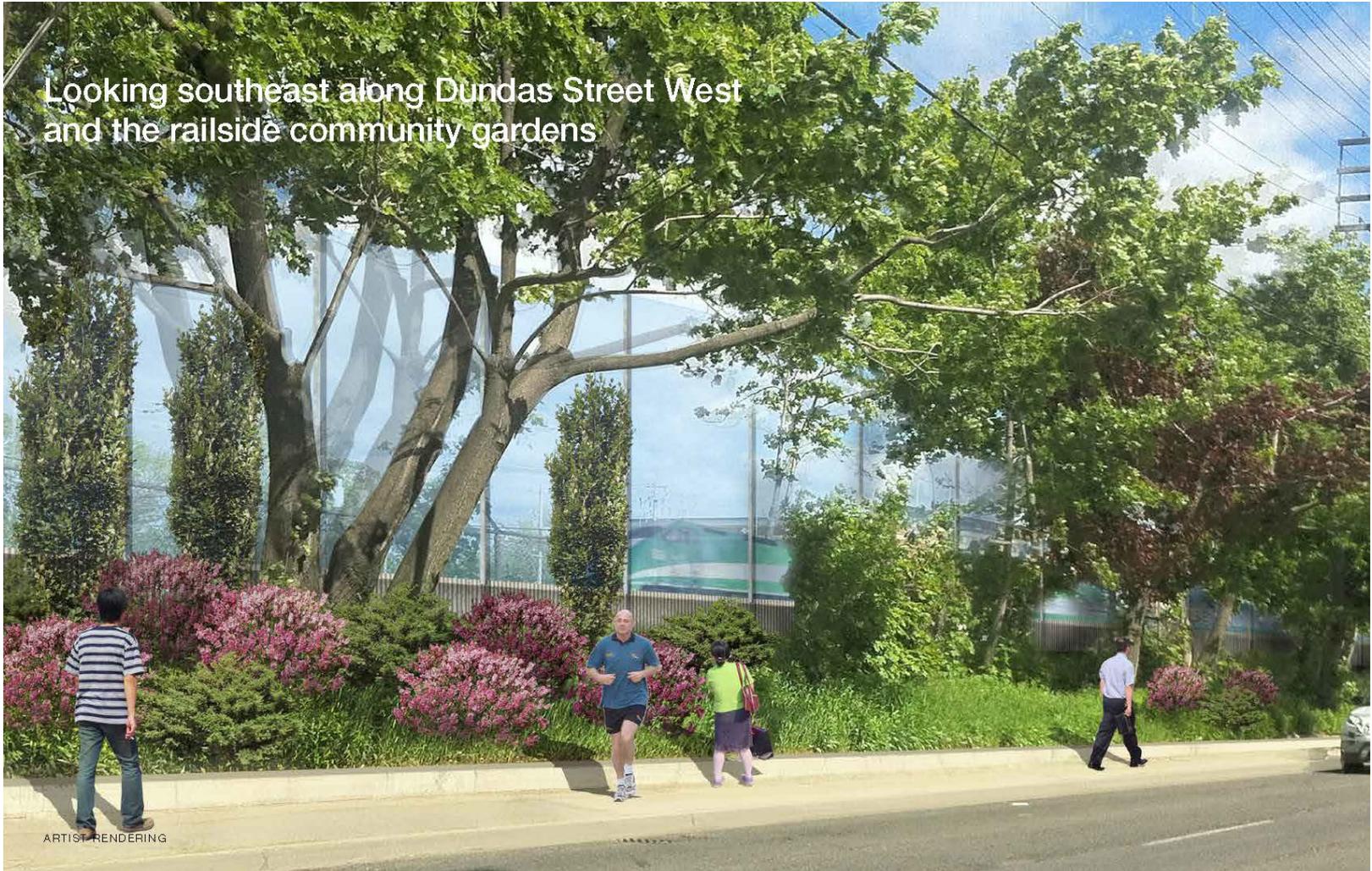


Design Examples in Your Community



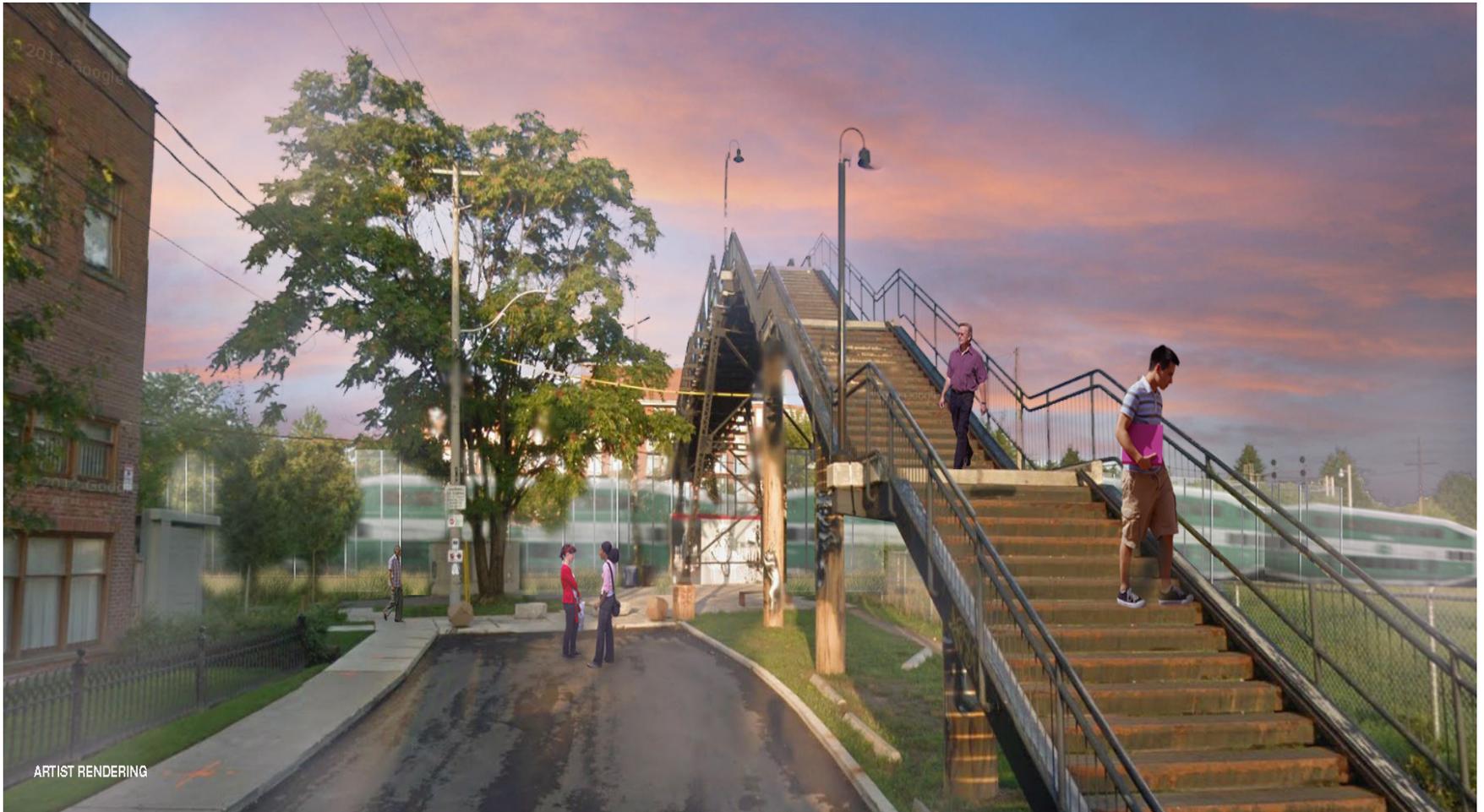
Dundas St. W – Railside Gardens

Looking southeast along Dundas Street West
and the railside community gardens



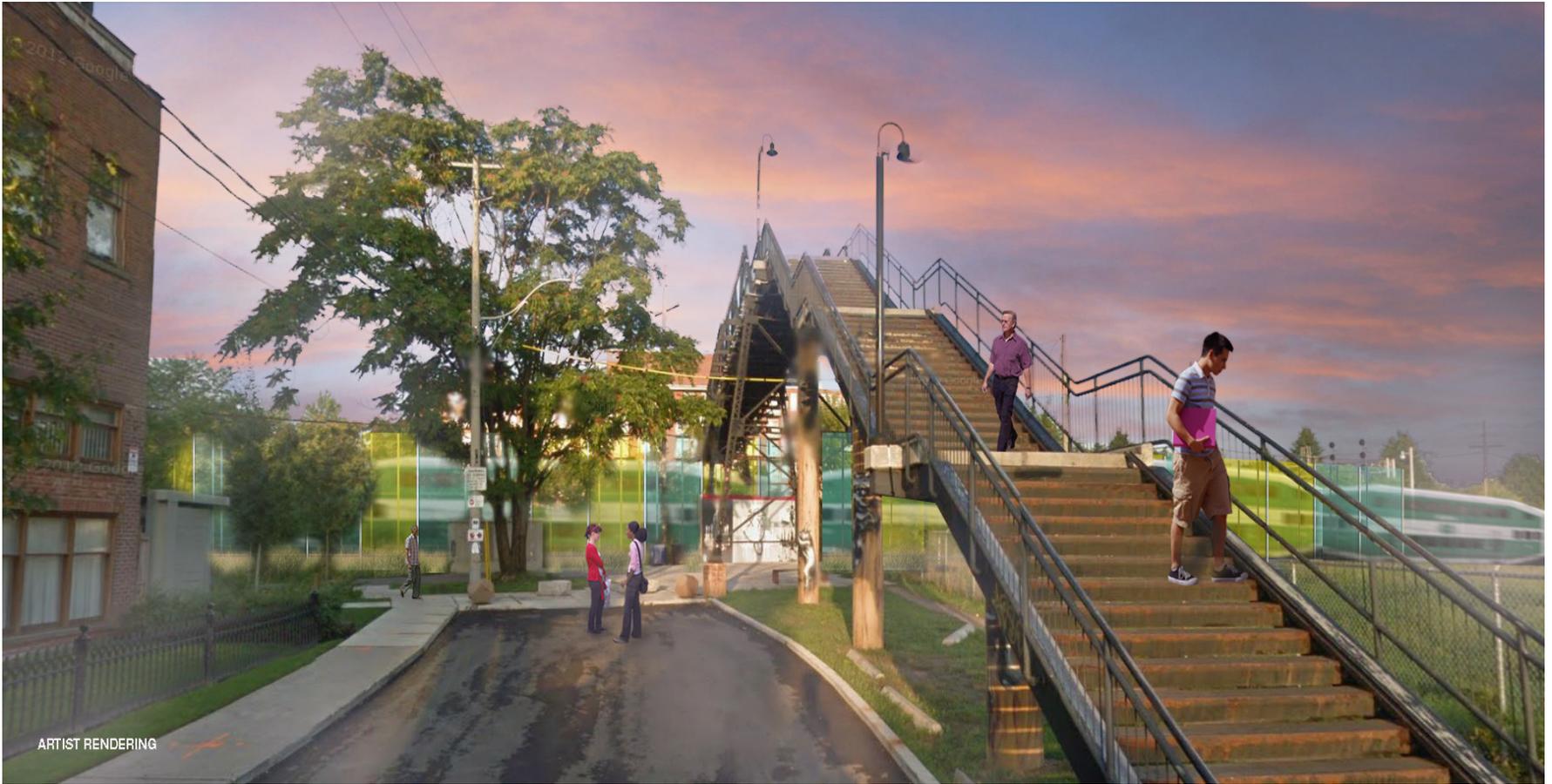
ARTIST RENDERING

West Toronto Rail Path at Wallace Ave. Bridge – Clear Acrylic



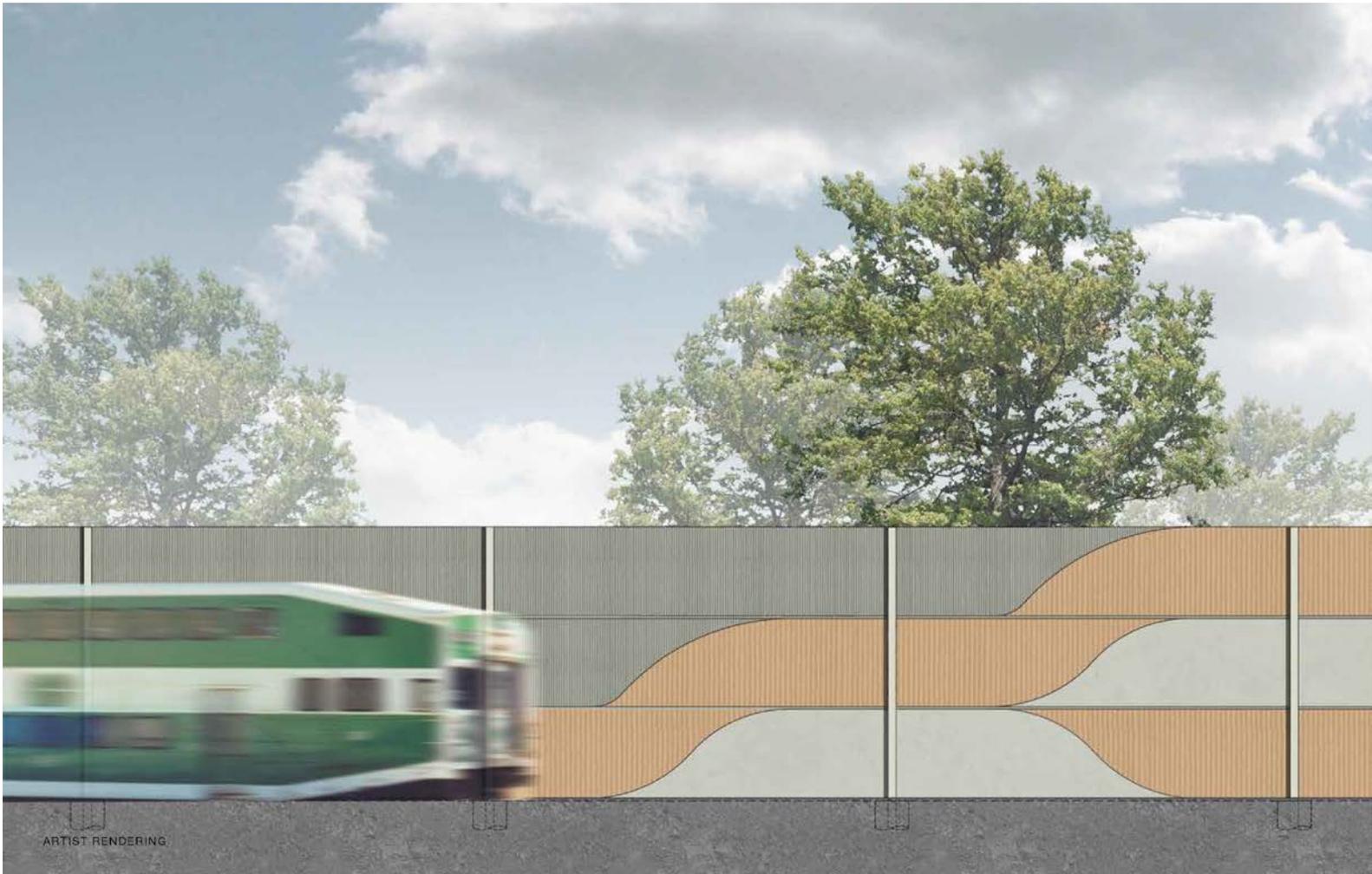
ARTIST RENDERING

West Toronto Rail Path at Wallace Ave. Bridge – Coloured Acrylic



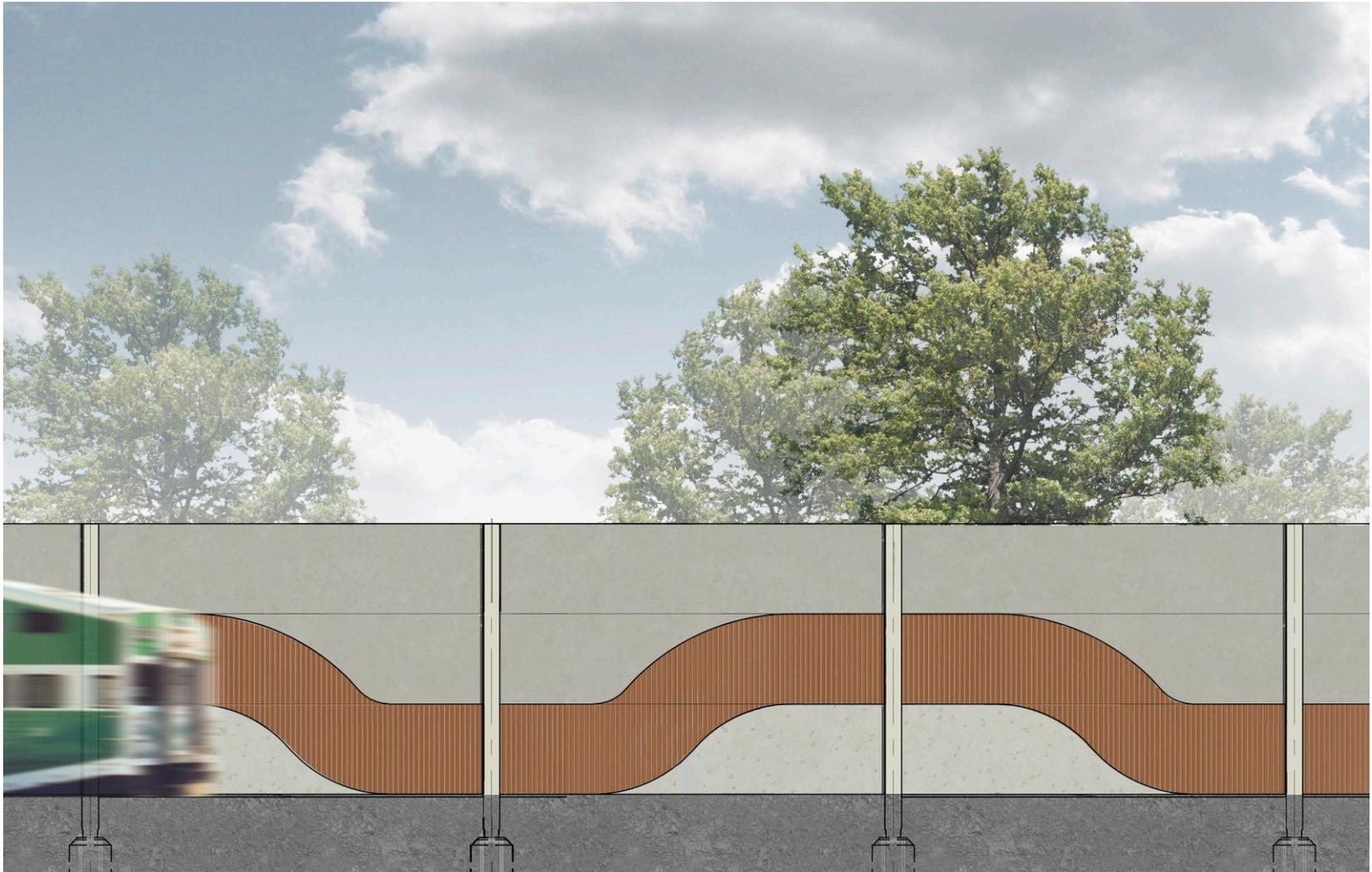
Design Examples in Your Community

Previous Rail Side View



Design Examples in Your Community

Refined Rail Side View



Final Wall Layout



Next Steps

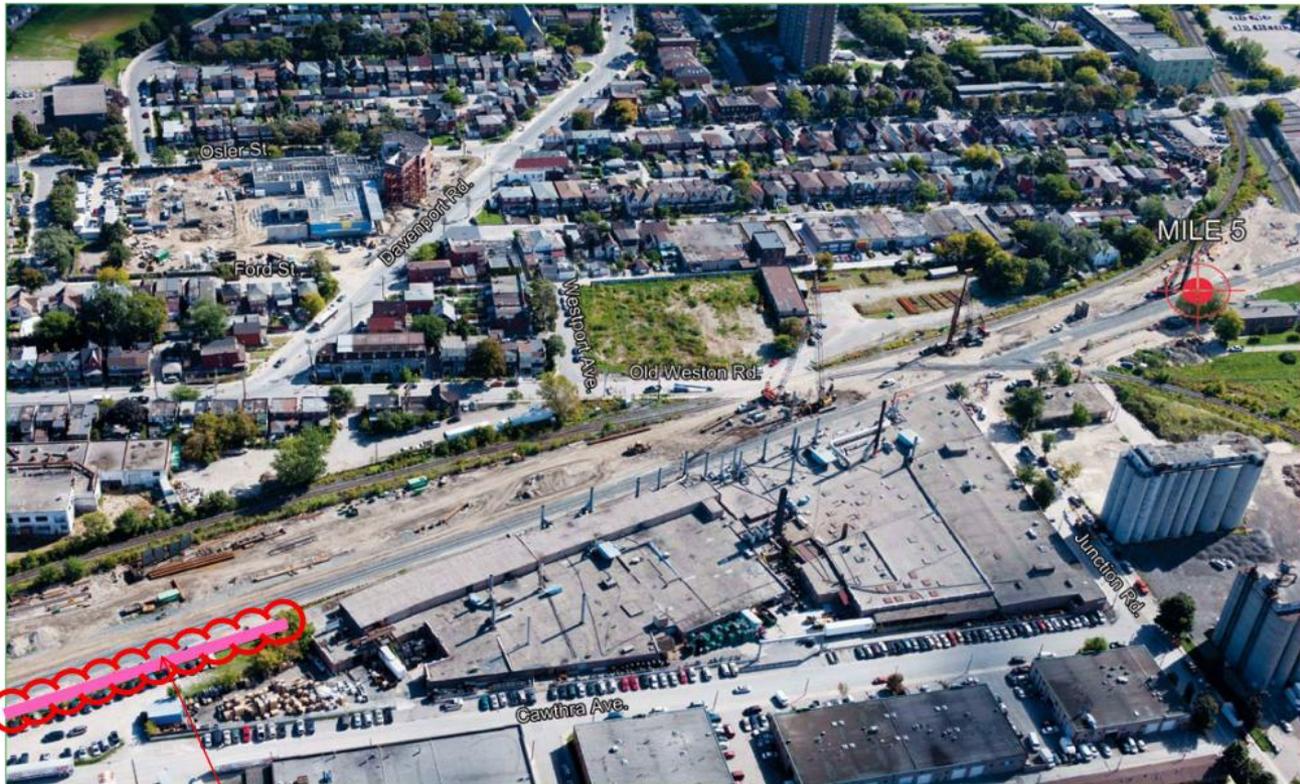
1. Noise wall detailed design begins.
2. Tender for construction to be issued in December.
3. Construction of noise walls expected to begin spring 2014 and wrap up in spring 2015. We will share the construction schedule once finalized (early 2014).
4. Public art commissioning will be shared with the community once the process is underway.
5. We will communicate our revised wall maintenance plan once it is finalized.

Impact on Noise With Electrification

- In March, GO Transit completed an assessment to identify noise wall requirements if the UP Express and GO commuter service were electrified.
- The study concluded that the decrease in average noise from trains for an electrified UP and GO service does not exceed 2 decibels.
- Most locations still have a 5 decibel increase.
- 85% of the noise walls along the GTS corridor would still be required to mitigate train noise.
- 100% of the noise walls from Bloor St. W. to Dupont St. would still be required for 2015 and Full Build Service. A small portion of walls between Dupont St. and St. Clair Ave. would not be required for full build service.

Impact on Noise With Electrification Walls Not Required For Full Build

Cawthra Avenue

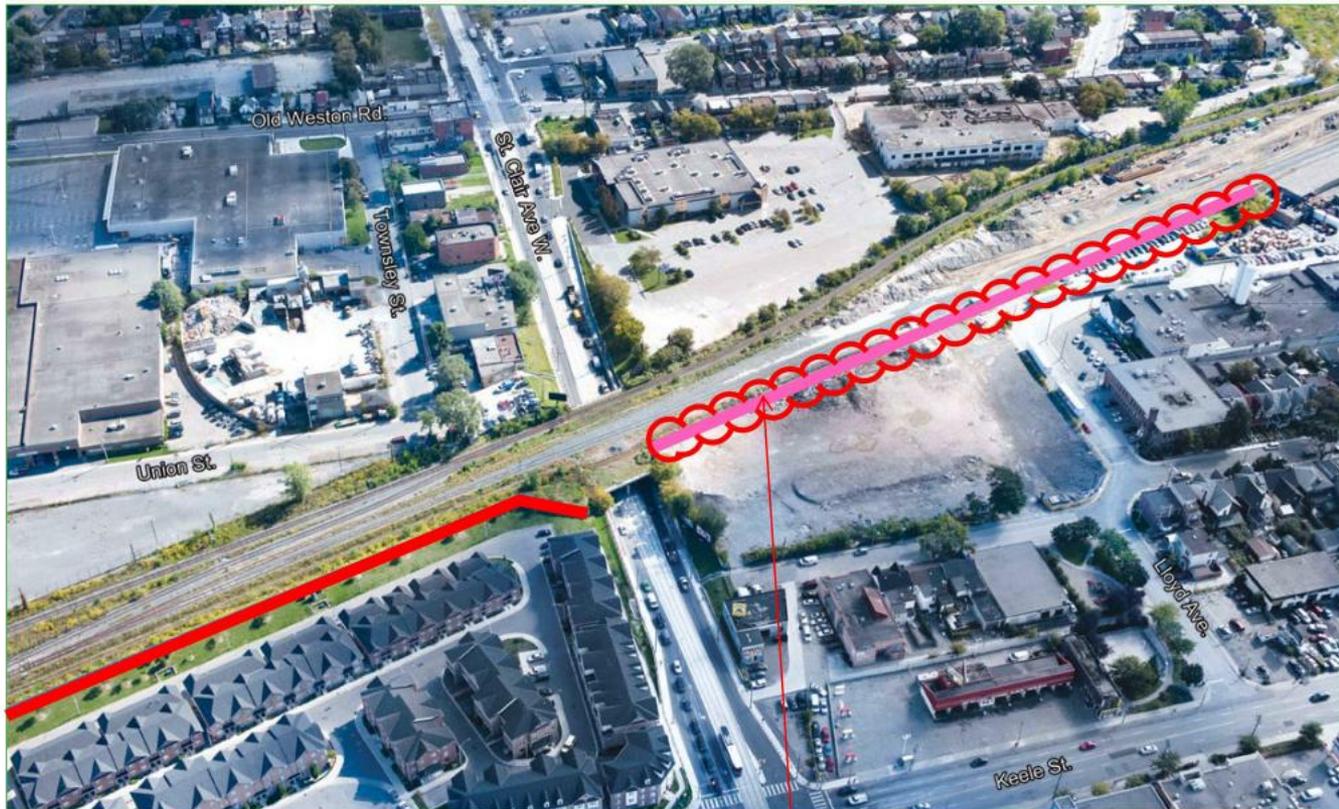


5 m height noise barrier

Based on reviewed sample:
Segment of noise barrier not required if UP Express & GO GTS -> Electric.

Impact on Noise With Electrification Walls Not Required For Full Build

St Clair Avenue West

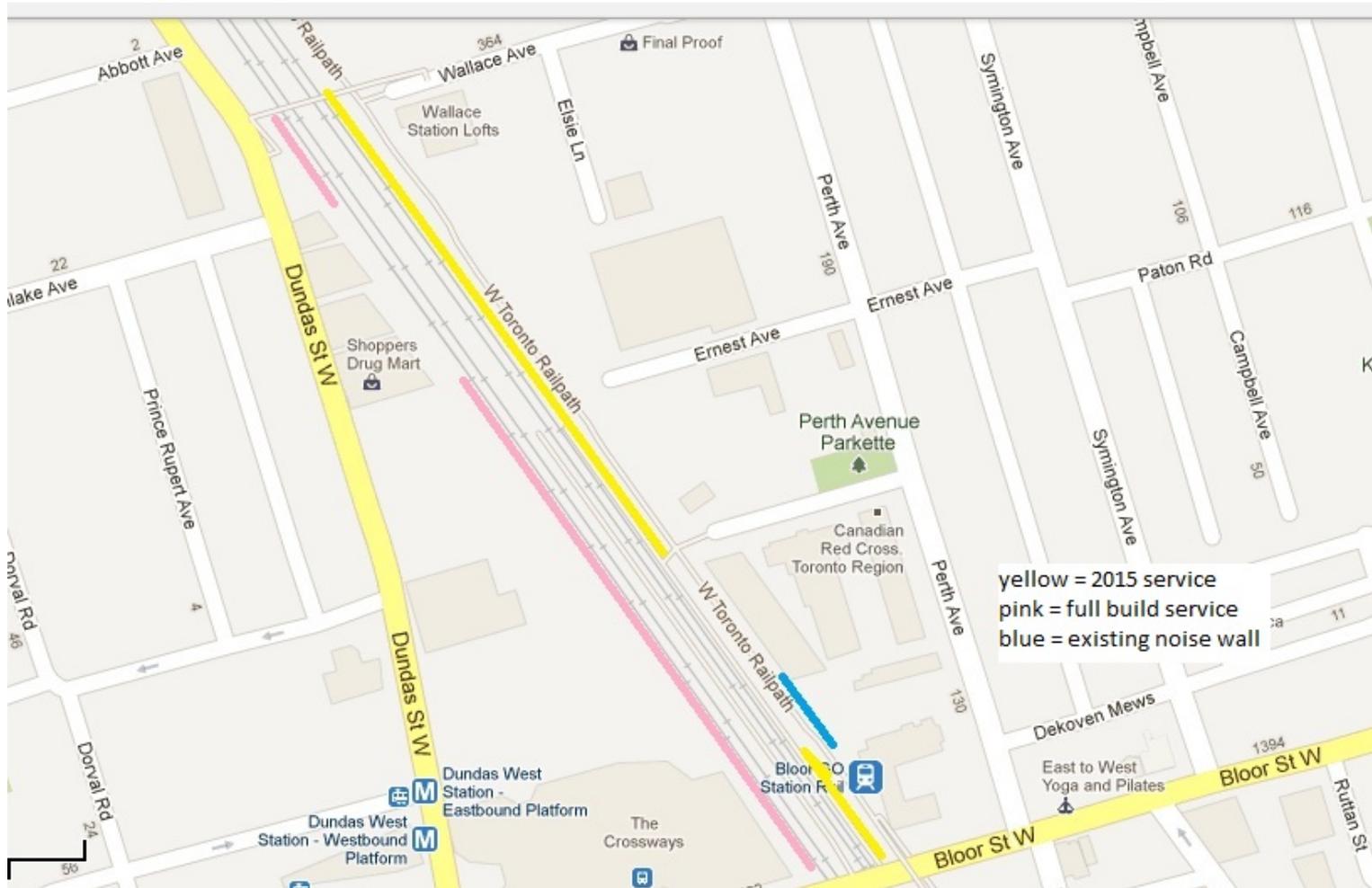


Existing noise barrier

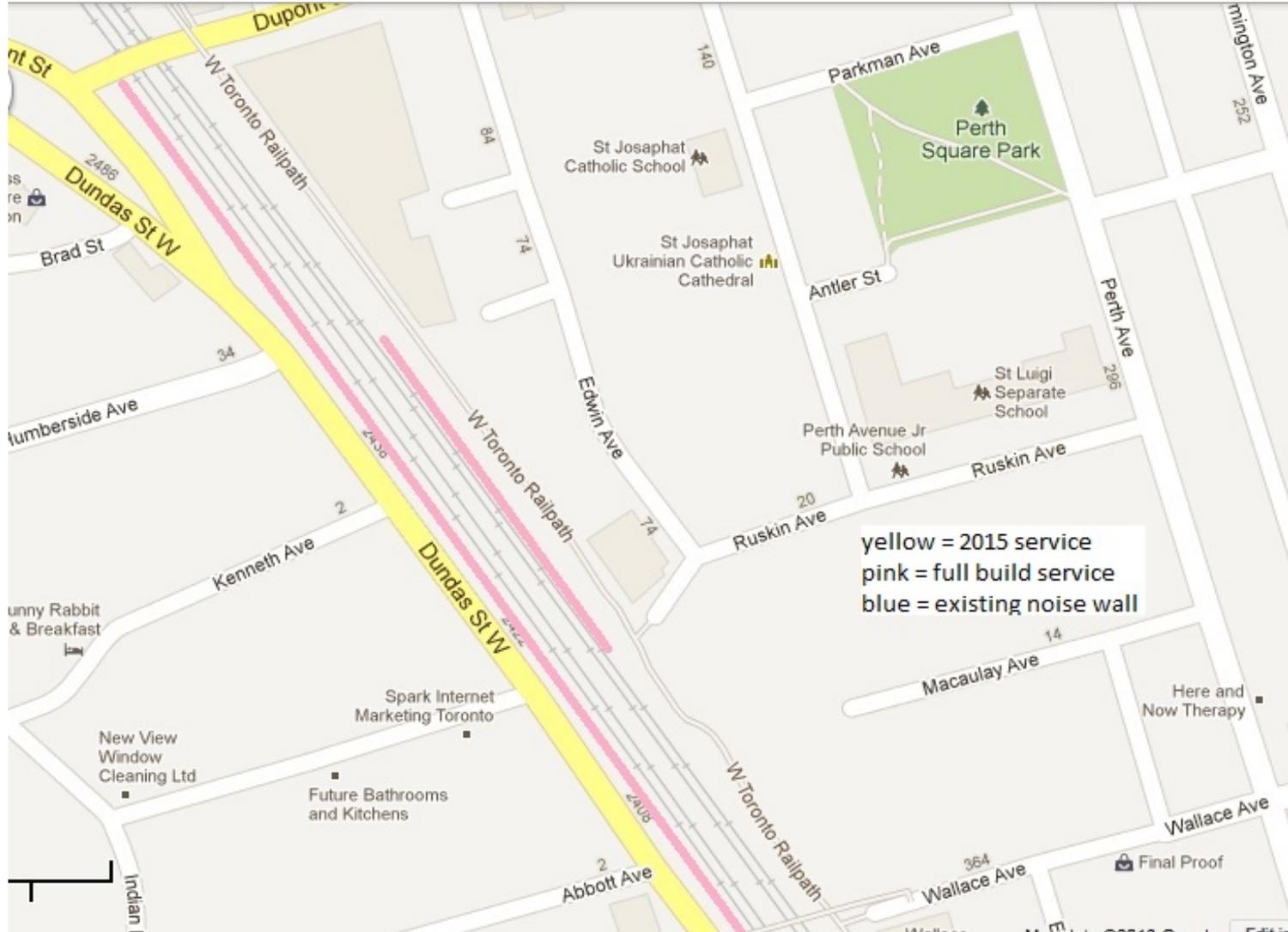
Based on reviewed sample:
Segment of noise barrier not required if UP Express & GO GTS -> Electric.

5 m height noise barrier

Bloor St. to Wallace Ave. Wall Locations



Wallace Ave. to Dupont St. Wall Locations



Dupont St. to St. Clair Ave. Wall Locations

